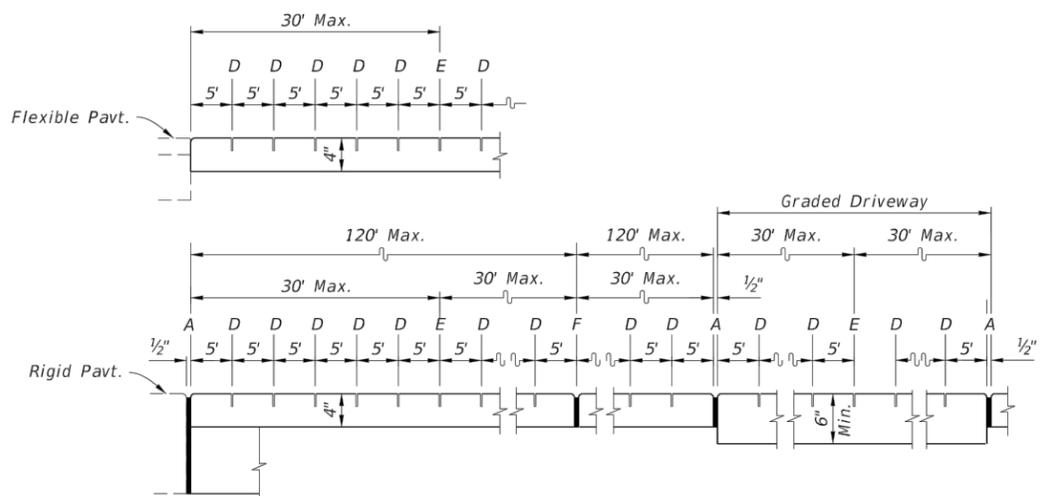


OPEN JOINTS



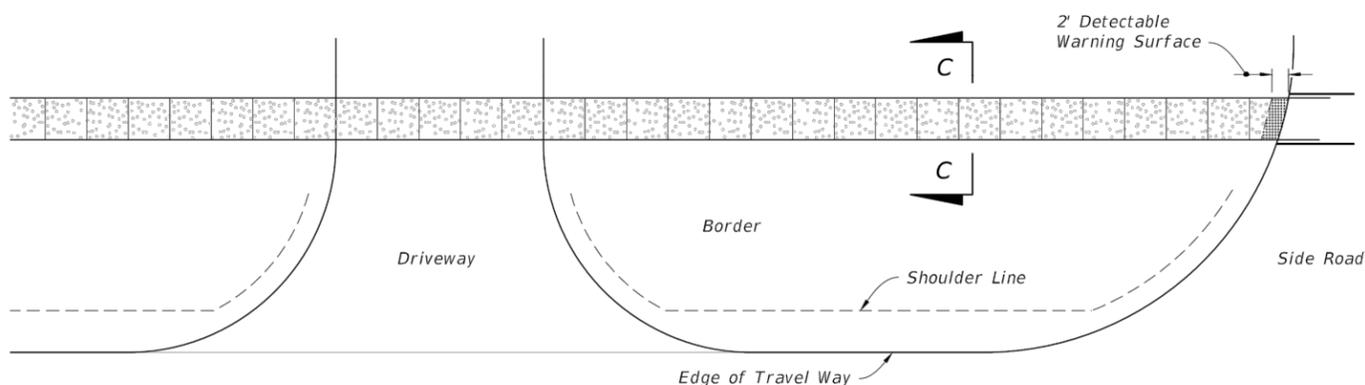
SAWED JOINTS

LONGITUDINAL SECTION

LEGEND:

- A- 1/2" Expansion Joints (Preformed Joint Filler) between the sidewalk and driveways, sidewalk-intersections, and all other fixed objects (e.g. drainage inlets and utility poles).
- B- 1/8" Dummy Joints, Tooled
- C- 1/8" Formed Open Joints
- D- 3/16" Saw Cut Joints, 1 1/2" Deep (within 96 hours) Max. 5' Centers
- E- 3/16" Saw Cut Joints, 1 1/2" Deep (within 12 hours) Max. 30' Centers Joint(s) Required When Length Exceeds 30'
- F- 1/2" Expansion Joint When Of Sidewalk Exceeds 120'. Intermediate locations when called for in the plans or at locations as directed by the Engineer.

SIDEWALK JOINTS

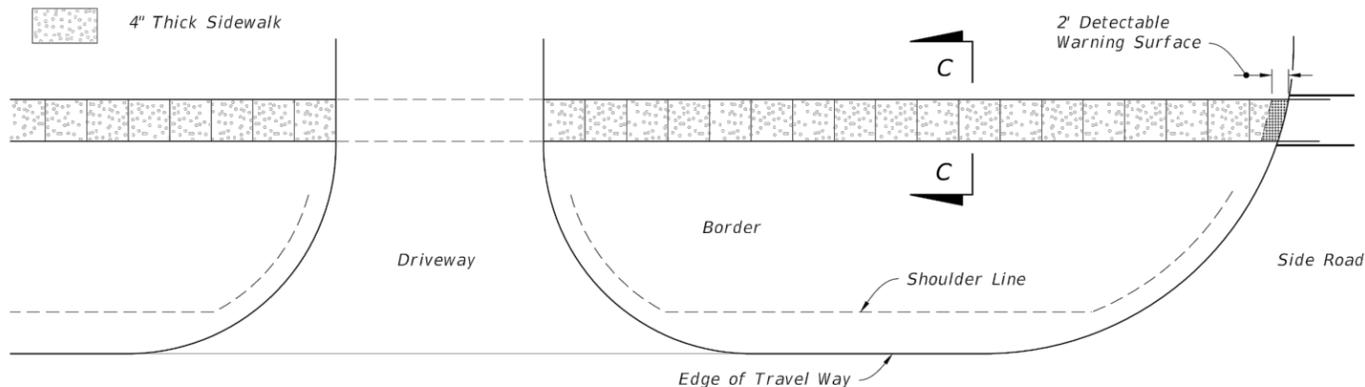


PLAN

CONTINUOUS SIDEWALK

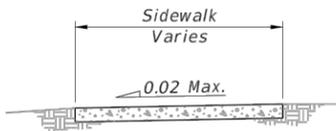
LEGEND:

4" Thick Sidewalk



PLAN

DISCONTINUOUS SIDEWALK



SECTION C-C

CONCRETE SIDEWALK ON FLUSH SHOULDER ROADWAYS

LAST REVISION 11/01/18	DESCRIPTION:	FDOT FY 2025-26 STANDARD PLANS	CONCRETE SIDEWALK	INDEX 522-001	SHEET 2 of 2
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P.O. Drawer 1000  
Arcadia, Florida, 34265  
(863) 494-4114



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525 OLYMPIA AVENUE, SUITE 5 PUNTA GORDA, FLORIDA 33950  
PHONE (352) 378-1444 WWW.GEORGEFYOUNG.COM  
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DATE

**Arcadia Stormwater and Flood Control**  
Special Details  
INCLUDES PORTIONS OF:  
SECTIONS 25, 26, 31, 36, TOWNSHIP 37S., RANGE 24, 25E.

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SHEET	CONTENTS
1	General Notes; Index Contents
2	General, TL-3 Guardrail - Installed Plan and Elevation
3	Low-Speed, TL-2 Guardrail - Installed Plan and Elevation
4	W-Beam and Thrie-Beam Panel Details
5	Post and Offset Block Details
6	Guardrail Sections - Heights and Adjacent Slopes
7	End Treatment - Approach Terminal Geometry, Parallel
8	End Treatment - Approach Terminal Geometry, Curbed and Double Faced
9	End Treatment - Trailing Anchorage
10	End Treatment - Component Details
11	End Treatment - Controlled Release Terminal (CRT) System
12	Layout for CRT System - Side Roads and Driveways
13	Approach Transition Connection to Rigid Barrier - General, TL-3
14	Approach Transition Connection to Rigid Barrier - General, TL-3 - Curb Connections
15	Approach Transition Connection to Rigid Barrier - Low-Speed, TL-2
16	Approach Transition Connection to Rigid Barrier - Low-Speed, TL-2 - Curb Connections
17	Approach Transition Connection to Rigid Barrier - Details
18	Approach Transition Connection to Rigid Barrier - Double Faced Guardrail
19	Layout to Rigid Barrier - Approach Ends
20	Layout to Rigid Barrier - Approach Ends with Double Faced Guardrail Layout to Rigid Barrier - Trailing Ends Trailing End Transition Connection to Rigid Barrier
21	Trailing End Transition Connection to Rigid Barrier - Curb Connections
22	Rub Rail Details
23	Pedestrian Safety Treatment - Pipe Rail
24	Modified Mount - Special Steel Post for Concrete Structure Mount; Modified Mount - Encased Post for Shallow Mount; Modified Mount - Frangible Leave-Out for Concrete Surface Mount
25	Barrier Delineators - Post Mounted; Clear Space - Reduced Post Spacing for Hazards; 5/8" Button-Head Bolt System

**GENERAL NOTES:**

1. **INSTALLATION:** Construct guardrail in accordance with Specification 536.

This Index, along with the plans and the manufacturers' drawings on the Approved Products List (APL), is sufficiently detailed for installation of General Guardrail, Low-Speed Guardrail, End Treatment assemblies, and their connecting options shown herein. This precludes requirements for shop drawing submittals unless otherwise specified in the plans.

2. **COMPATIBILITY:** The General Guardrail in this Index is based on the Midwest Guardrail System (MGS) design, with an approximate height of 31" at the top of the Panel (2'-1" mounting height at vertical  $\bar{C}$  of Panel) and a midspan panel splice as shown on Sheet 2. Guardrail components included on the APL, which are compatible with this Index, may also be identified as 31" or MGS Guardrail.

3. **STANDARD COMPONENTS:** Standard guardrail components, including posts, panels, and bolt systems, are based on the Task Force 13 Publication: Guide to Roadside Hardware Components (<http://tf13.org/Guides/componentGuide/>).

4. **BUTTON-HEAD BOLTS:** Install Button-Head Bolts where indicated using bolts, nuts, and washers as defined on Sheet 25. Place washers under nuts against timber posts. Washers are not required at steel post flanges and panel lap splices. Do not place washers between bolt heads and panels, except where otherwise shown in this Index.

5. **HEX-HEAD BOLTS:** Install Hex-Head Bolts where indicated using bolts, nuts, and washers in accordance with material properties of Specification 967. Place washers under nuts.

6. **MISCELLANEOUS ASPHALT PAVEMENT:** Install Miscellaneous Asphalt Pavement where indicated with a tolerance of  $\pm 1/2$ " depth and in accordance with Specification 339.

7. **ADJACENT SIDEWALKS & SHARED USE PATHS:** When guardrail posts are placed within 4'-0" of a sidewalk or shared use path, use timber posts, or use steel posts only if treated with Pipe Rail as shown on Sheet 23.

When timber posts are used, one of the following safety treatments is required for the bolt(s) protruding from the back face of the posts:

- a. After tightening the nut, trim the protruding post bolt flush with the nut and galvanize per Specification 562.
- b. Use post bolts 15" in length and countersink the washer and nut between 1" and 1 1/2" deep into the back face of the post.
- c. Use 15" post bolts with sleeve nuts and washers.

When End Treatment posts are within 4'-0" of a sidewalk or shared use path, steel posts are not permitted within the End Treatment segment. Terminate the Pipe Rail outside of End Treatment segments, as noted per Sheet 23.

8. **NESTED W-BEAM:** Where called for in the plans, install two W-Beam Panels mounted flush per location, securing all panels with Button-Head Bolts threaded through aligned slots and holes. 2" Button-Head Bolts are permitted for panel splice locations.

9. **CONNECTION TO RIGID BARRIER:** The connections to Rigid Barrier in this Index only apply to newly constructed bridge Traffic Railing and Concrete Barrier or where the complete Approach Transition Connection to Rigid Barrier shown herein can be installed without conflicting with existing Traffic Railings, structures, or approach slabs.

For connecting guardrail to existing bridge Traffic Railings, see Indexes 536-002, 521-404, and 521-405.

10. **CONNECTION TO EXISTING GUARDRAIL:** Where a transition to existing guardrail at 27" height is required, linearly transition the new guardrail height over a distance ranging from 25'-0" to 31'-3". Height transitions must occur outside of End Treatment and Approach Transition segments.

Provide an immediate transition to the required midspan panel splice using the available panel options on Sheet 4 (9'-4 1/2" or 15'-7 1/2" panel). Alternatively, this transition to midspan panel splice may be achieved by installing a single reduced post spacing of 3'- 1 1/2" within the new guardrail, immediately adjacent to the connection location.

11. **PLANS CALLOUTS:** Begin/End Station labels are shown throughout this Index as they correspond to the station and offset callouts specified in the plans.

In the plans, Begin/End Guardrail Station refers to the General TL-3 Guardrail Pay Item, and it may be abbreviated as Begin/End GR. Station. Where the Low-Speed TL-2 Guardrail Pay Item is specifically required, the callout in the plans will then specify Begin/End TL-2 GR. Station.

12. **QUANTITY MEASUREMENT:** Measure guardrail and corresponding components as defined in Specification 536. The Guardrail length is measured along the centerline of installed Panels, between the points labeled Begin/End Guardrail Station shown on the following Index Sheets and defined in the plans (typically measured from the  $\bar{C}$  of the panel's post bolt slots at the approach/trailing ends).

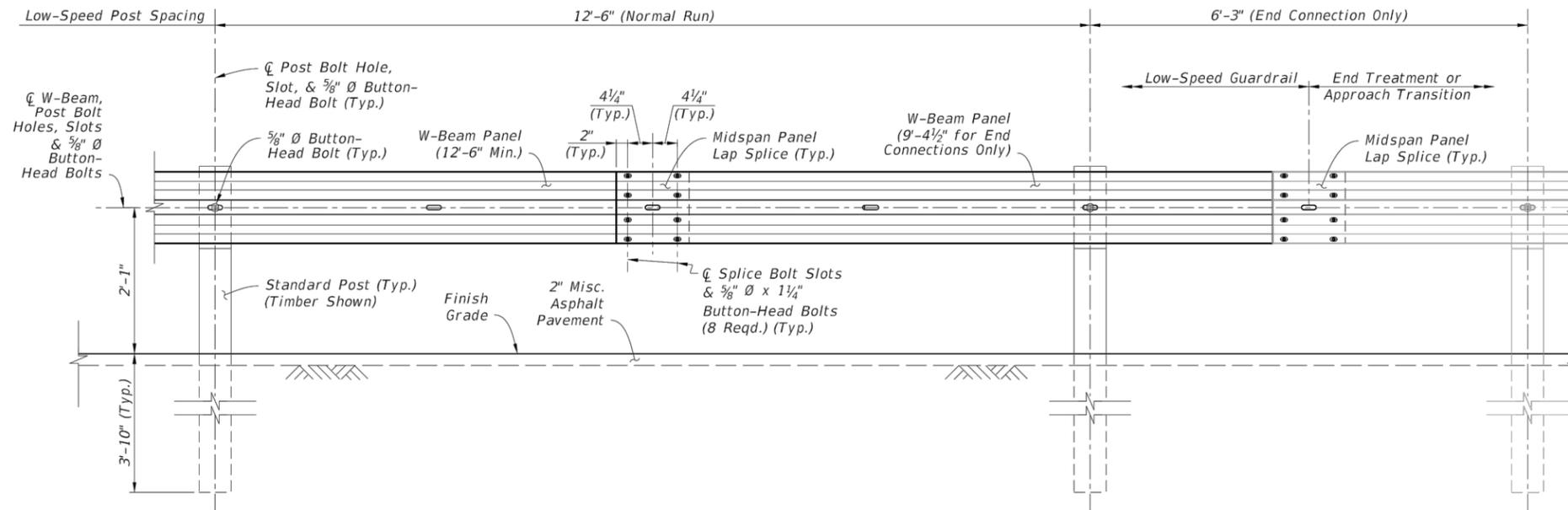
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LAST REVISION 11/01/23	REVISION	DESCRIPTION:	 <b>FY 2025-26</b> STANDARD PLANS	<b>GUARDRAIL</b>	INDEX 536-001	SHEET 1 of 25
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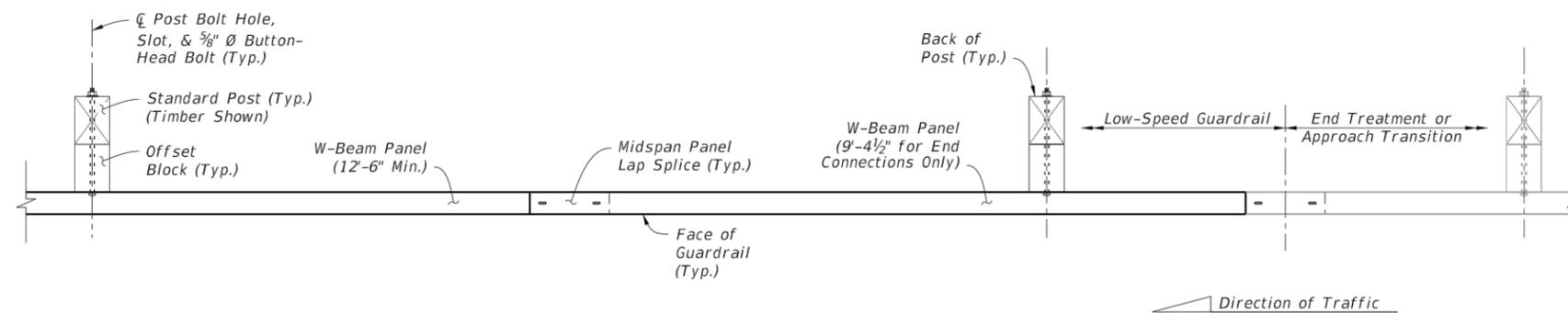
REVISED	NO.	BY	DATE	DESCRIPTION	DESIGN	INITIALS	DATE	PREPARED FOR:	  <p><b>George F. Young, Inc.</b> 525 OLYMPIA AVENUE, SUITE 5 PUNTA GORDA, FLORIDA 33950 PHONE (352) 378-1444 WWW.GEORGEFYOUNG.COM ENGINEERING CERTIFICATE OF AUTHORIZATION NUMBER 21 CIVIL, TRANSPORTATION, SUBSURFACE &amp; STRUCTURAL ENGINEERING ECOLOGICAL   GIS   PLANNING   SURVEYING ST. PETERSBURG • LAKEWOOD RANCH • TAMPA • GAINESVILLE • LAKE WALES • PUNTA GORDA</p>	<p style="text-align: center;"><b>Arcadia Stormwater and Flood Control</b></p> <p style="text-align: center;">Special Details</p> <p>INCLUDES PORTIONS OF: SECTIONS 25, 26, 31, 36, TOWNSHIP 37S., RANGE 24, 25E.</p>	<p>JOB NO. 21Y01018LC</p> <p>SHEET NO. SD42</p>
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					DRAWN BY	PCS		P.O. Drawer 1000			
					CHECKED BY	MP		Arcadia, Florida, 34265			
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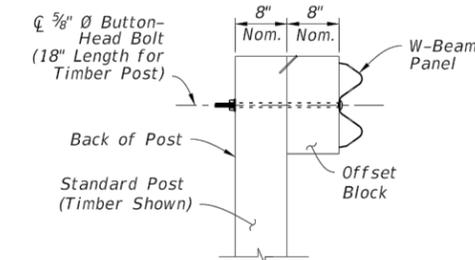




LOW-SPEED GUARDRAIL  
INSTALLED ELEVATION



INSTALLED PLAN



INSTALLED SECTION

NOTES:

1. GENERAL: Install the Low-Speed Guardrail configuration where indicated in the plans. Low-Speed Guardrail may include tapered segments if called for in the plans.  
  
Use 12'-6" or 25'-0" W-Beam Panels for normal spans, and use 9'-4 1/2" Panels for end connections to adjoining segments as shown. A single 6'-3" Panel may be used at the end of the Low-Speed Guardrail run along with a single reduced 6'-3" post spacing to meet the nominal Begin/End Guardrail Sta. required.
2. MIDSPAN PANEL LAP SPLICE: For proper structural function, place all Lap Splices at midspan unless otherwise indicated.  
  
Lap the Panels with the Splice Ridge oriented downstream of the final Direction of Traffic in the nearest traffic lane. For reverse lane conditions, orient the Splice Ridge downstream of the lane direction with the highest traffic volume. Orienting Lap Splices for Temporary Traffic Control phasing is not required.
3. CONNECTION DETAILS: Connections to End Treatments, Approach Transitions, or other segment types are defined in the following Index Sheets, APL Drawings, or the plans.
4. W-BEAM PANEL DETAILS: See Sheet 4.
5. POST & OFFSET BLOCK DETAILS: See Sheet 5.
6. GUARDRAIL SECTIONS: For Sections showing typical mounting heights, grading, and lateral offsets in relation to adjacent roadway features, see Sheet 6.
7. MODIFIED MOUNTS: Where concrete structures, concrete sidewalk, or shallow depth conditions are encountered, see Sheet 24 for additional post mounting options.
8. RESTRICTIONS: Low-Speed Guardrail segments are not permitted for use with items including, but not limited to, Double Faced W-Beam, Deep Posts at Slope Breaks, Raised Curb, Pipe Rail, and/or Rub Rail.

LOW-SPEED, TL-2 GUARDRAIL DETAILS

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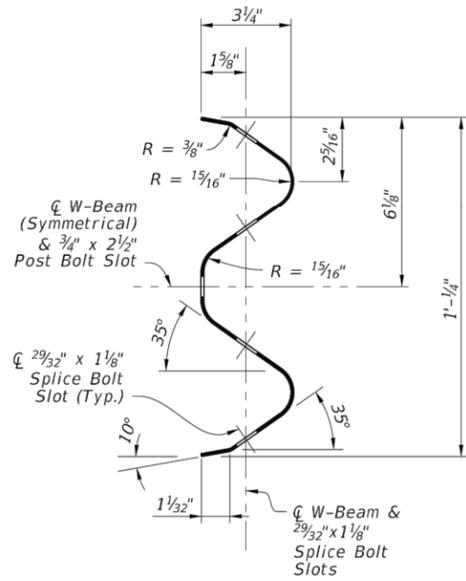


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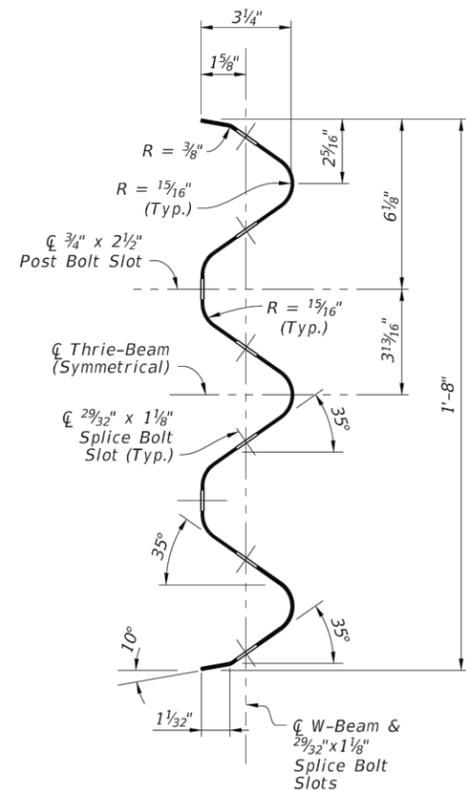
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INCLUDES PORTIONS OF:  
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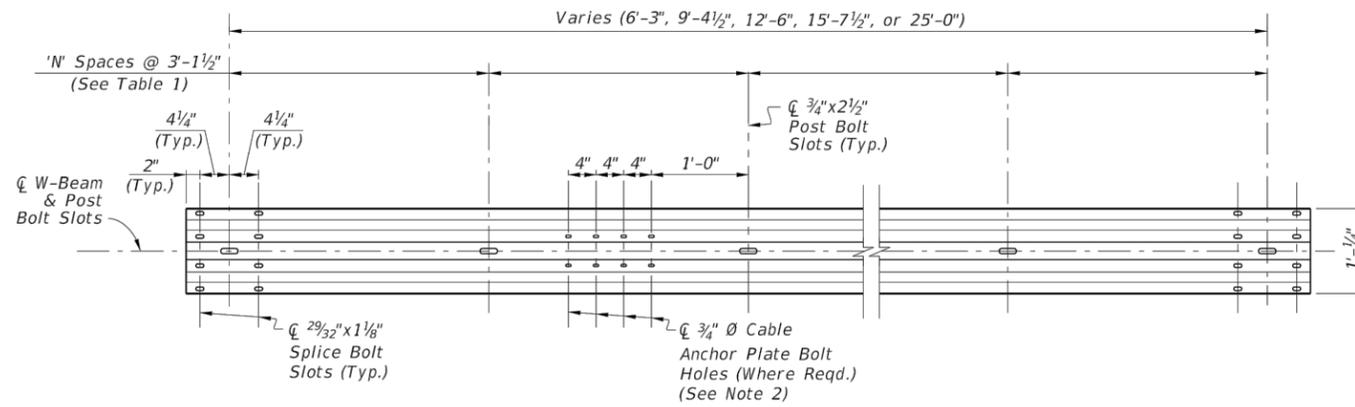
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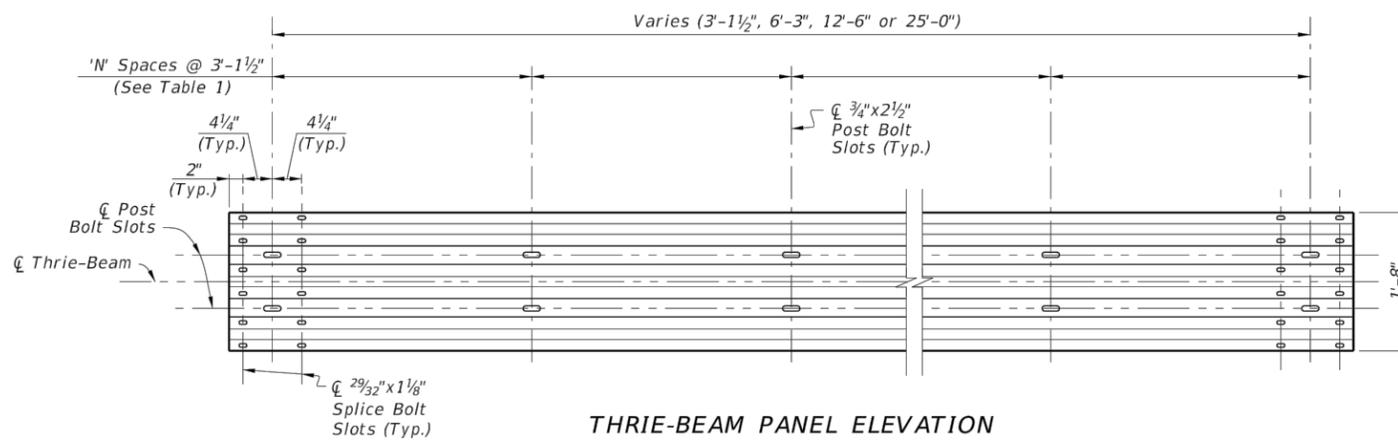
W-BEAM PANEL SECTION



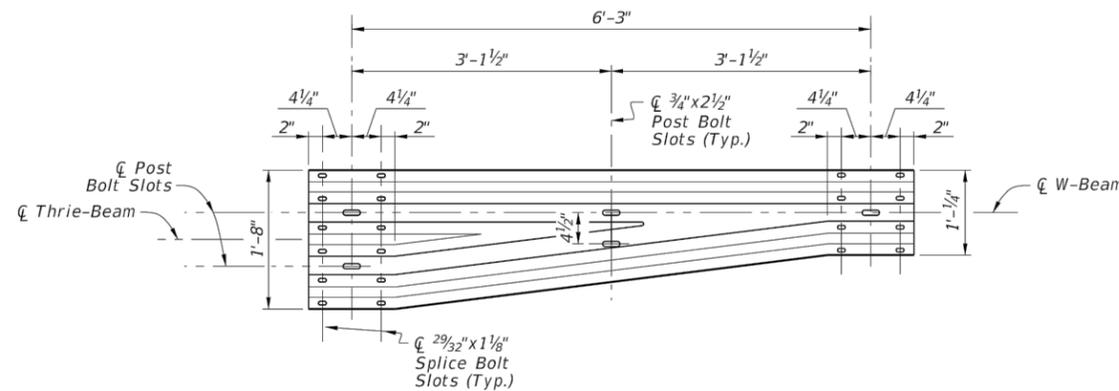
THRIE-BEAM PANEL SECTION



W-BEAM PANEL ELEVATION



THRIE-BEAM PANEL ELEVATION



THRIE-BEAM TRANSITION PANEL ELEVATION  
(Reverse Direction Similar by Opposite Hand)

PANEL SUMMARY TABLE:

Panel Type	Number of Spaces 'N'	Gauge
6'-3" W-Beam	2	12
9'-4 1/2" W-Beam	3	12
12'-6" W-Beam	4	12
15'-7 1/2" W-Beam	5	12
25'-0" W-Beam	8	12
3'-1 1/2" Thrie-Beam	1	10
6'-3" Thrie-Beam	2	12
12'-6" Thrie-Beam	4	12
25'-0" Thrie-Beam	8	12
Thrie-Beam Trans.	2	10

NOTES:

- MATERIALS:**  
Use corrugated steel panels in accordance with Specification 967 and made from either Class A, 12 gauge steel or Class B, 10 gauge steel as specified in the 'Panel Summary Table' above.
- CABLE ANCHOR PLATE BOLT HOLES:**  
Include 3/4" Ø Cable Anchor Plate Bolt Holes only where required for installation of the Cable Anchor Plate shown on Sheet 9, 10, & 11.  
  
29/32" x 1 1/8" slots may substitute for the 3/4" Ø holes shown.

W-BEAM AND THRIE-BEAM PANEL DETAILS

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**FDOT** FY 2025-26  
STANDARD PLANS

GUARDRAIL

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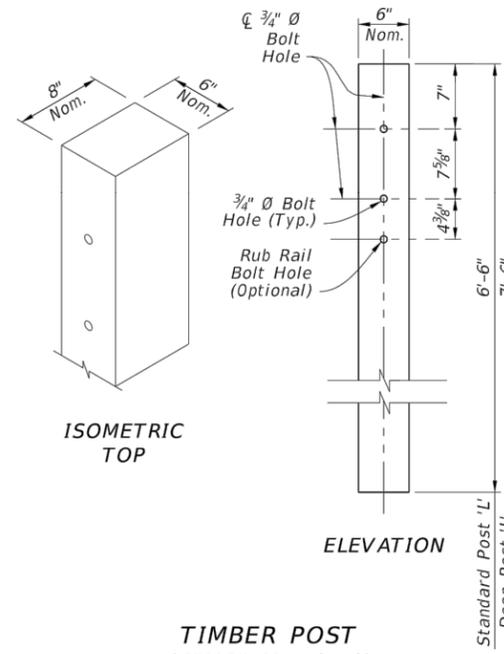


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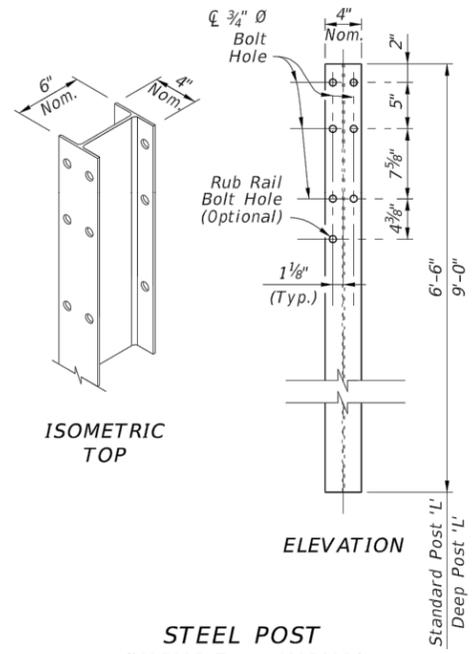
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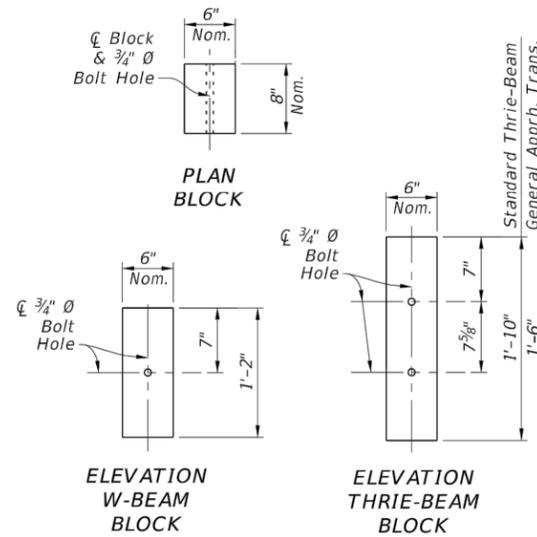
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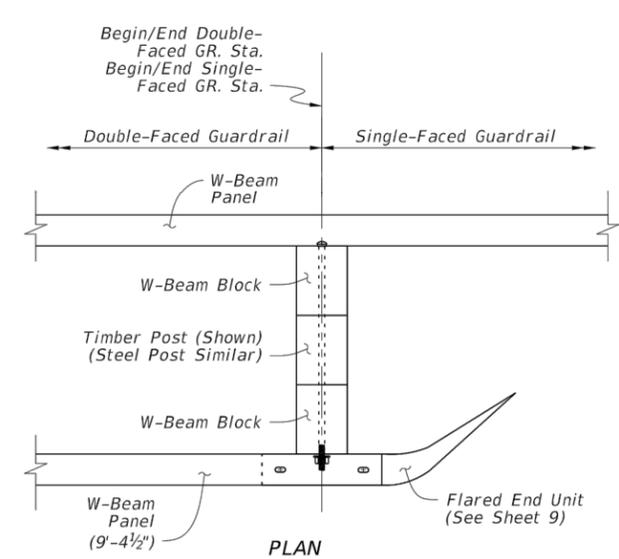
**TIMBER POST**  
(6"X8" Nominal)



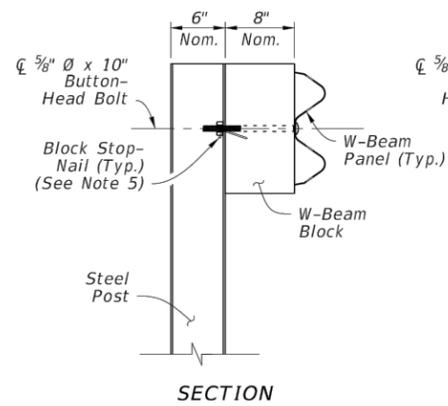
**STEEL POST**  
(W6X8.5 or W6X9)



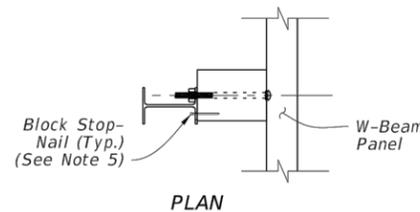
**TIMBER OFFSET BLOCK**  
(6"X8" Nominal)



**SINGLE-FACED / DOUBLE-FACED**  
**GUARDRAIL CONNECTION**

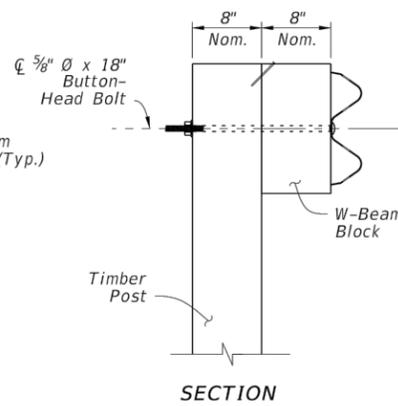


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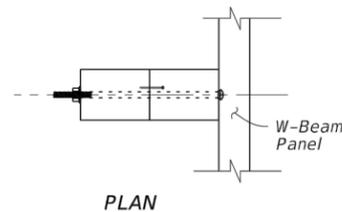


**PLAN**

**SINGLE-FACED**  
**W-BEAM**  
**STEEL POST**

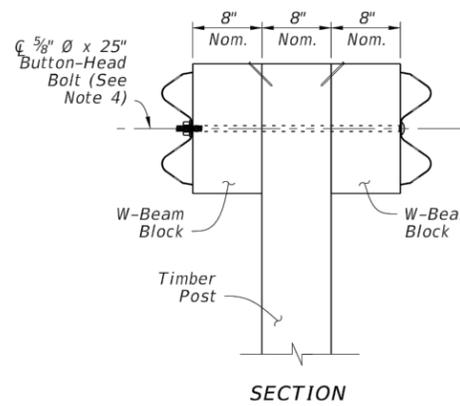


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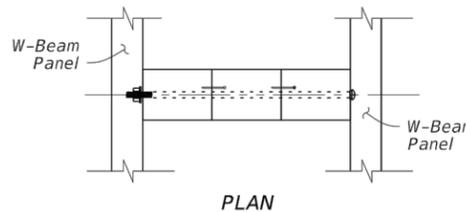


**PLAN**

**SINGLE-FACED**  
**W-BEAM**  
**TIMBER POST**

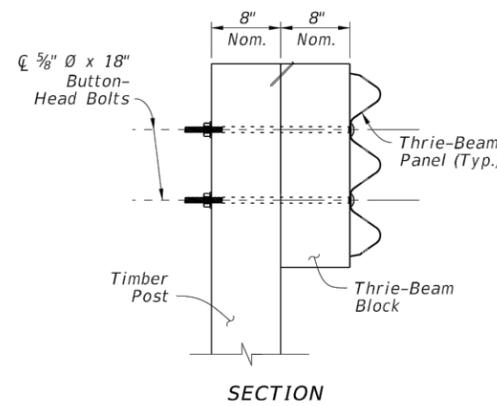


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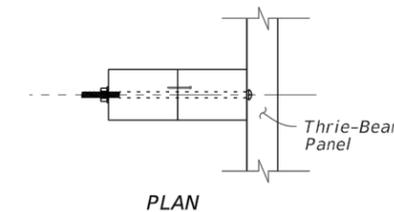


**PLAN**

**DOUBLE-FACED W-BEAM**  
**TIMBER POST**  
(Thrie-Beam Similar)  
(Steel Post Similar)



**SECTION**



**PLAN**

**THRIE-BEAM**  
**TIMBER POST**  
(Steel Post Similar)

**NOTES:**

- STANDARD POSTS:** Where Standard Posts are called for in this Index, use either a Timber Post or Steel Post at the Length, 'L', shown for Standard Posts. Use a single post material type consistently per each run of guardrail. Only where specified in the Plans, use the Deep Post 'L' for Slope Break Conditions as shown on Sheet 6.
- OFFSET BLOCKS:** For each Panel type, install the corresponding Offset Block type as shown. For General, TL-3 (Single Faced) Approach Transitions only, use the 1'-6" Thrie-Beam Block (See Sheet 13).
- BOLT HOLES:** 3/4" Ø Bolt Holes shown in posts within this Index may be substituted with 13/16" Ø Bolt Holes.
- DOUBLE FACED GUARDRAIL:** Orient Post Bolts with the Button-Head located on the side nearest the traffic lane. The bolt's threaded portion is not permitted to extend beyond 3/4" from the face of the tightened nut; trim the threaded portion as needed and galvanize in accordance with Specification 562.
- BLOCK STOP-NAIL:** Drive one nail per Standard Offset Block as shown to prevent Block rotation. Use steel 3 1/2" Type 16d nails with ASTM A153 hot-dip galvanization. For steel posts, drive the nail through the unused flange Bolt Hole and bend the nail so its head contacts the flange.
- MATERIALS:** Use timber and steel posts and offset blocks in accordance with Specification 967. Composite offset blocks may be substituted as approved on the APL. Use a single offset block type consistently per each run of guardrail.

**POST AND OFFSET BLOCK DETAILS**

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**FY 2025-26**  
**STANDARD PLANS**

**GUARDRAIL**

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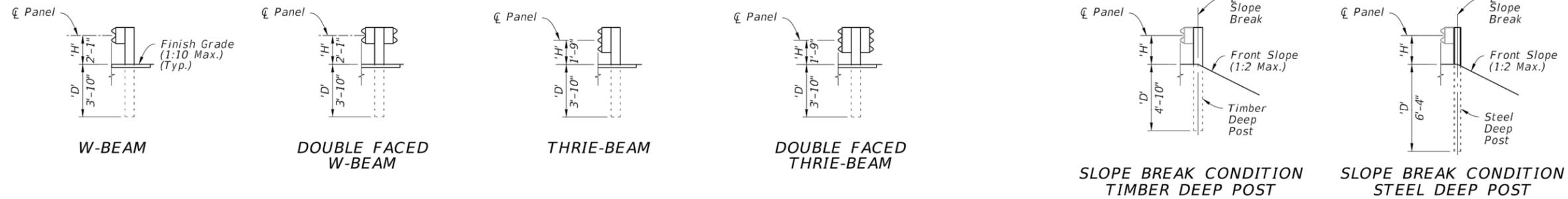
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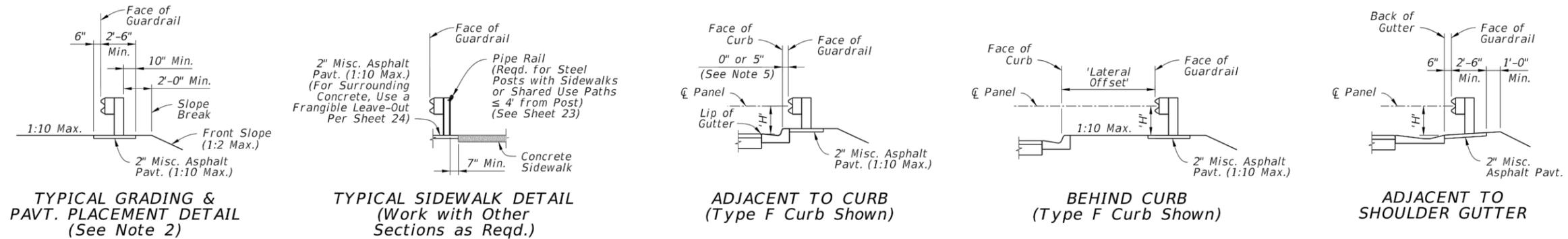
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JOB NO.	21Y01018LC
SHEET NO.	SD46

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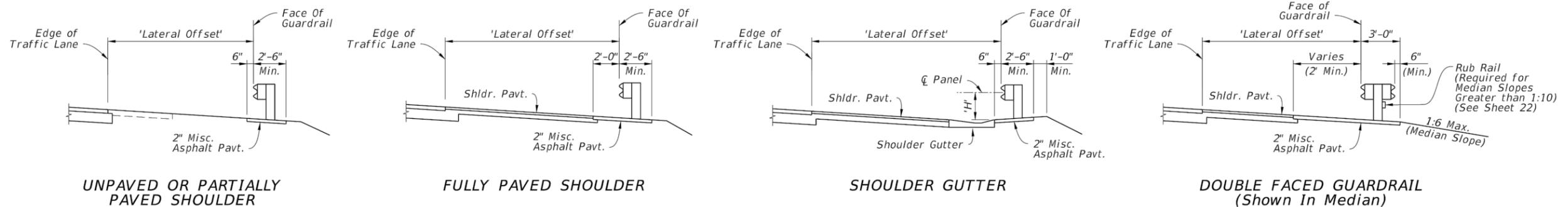


GUARDRAIL TYPES - MOUNTING HEIGHTS & POST DEPTHS



GUARDRAIL SECTIONS - TYPICAL

GUARDRAIL SECTIONS - CURB & GUTTER



GUARDRAIL SECTIONS - SHOULDERS

GUARDRAIL HEIGHT SUMMARY TABLE:			
Type:	Min. Depth 'D':	Mounting Height 'H':	Post Length 'L':
W-Beam (Single and Double Faced)	3'-10"	2'-1"	6'-6"
Thrie-Beam (Single and Double Faced)	3'-10"	1'-9"	6'-6"
Timber Deep Post	4'-10"	See Above	7'-6"
Steel Deep Post	6'-4"	See Above	9'-0"

NOTES:

1. GUARDRAIL SECTIONS: Construct Sections as indicated in the plans. The details shown herein depict W-Beam Guardrail, but are applicable to the other defined Guardrail Types placed at the corresponding height, 'H'. Use components per Sheets 4 & 5. Steel and timber post types are interchangeable unless otherwise defined. The 1:10 Max. cross slope shown is the maximum slope permitted for proper guardrail function, but project-specific cross slope requirements are governed by additional design criteria, per the plans.
2. TYPICAL GRADING & PAVEMENT PLACEMENT DETAIL: Construct features as depicted except where superseded by specific Guardrail Sections or the plans. Place the Slope Break a Minimum of 2' behind the post. For Deep Posts, the slope break may be placed at the  $\bar{C}$  Post with the 2" Miscellaneous Asphalt Pavement omitted.
3. SLOPE BREAK CONDITION: Install Deep Posts only where called for in the plans. Deep Posts are only permitted where post spacing is 6'-3" or less.
4. LATERAL OFFSETS: The Lateral Offsets shown are governed by the station and offset call outs for Face of Guardrail, as shown in the plans.
5. ADJACENT TO CURB: Place the Face of Guardrail consistently offset either flush with the Face of Curb or 5" behind the Face of Curb, as indicated by the plans station and offset callout. For offset changes, transition the Face of Guardrail as shown in the plans.

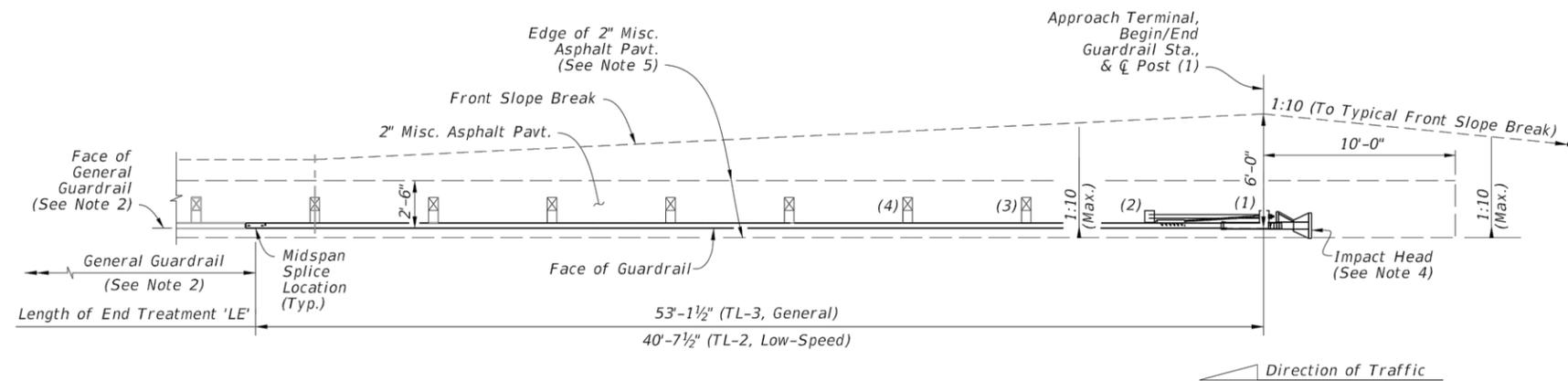
GUARDRAIL SECTIONS

LAST REVISION 11/01/23	DESCRIPTION:	FDOT	FY 2025-26 STANDARD PLANS	GUARDRAIL	INDEX 536-001	SHEET 6 of 25
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REVISED	NO.	BY	DATE	DESCRIPTION	INITIALS	DATE	PREPARED FOR:	 City Of Arcadia P.O. Drawer 1000 Arcadia, Florida, 34265 (863) 494-4114	 George F. Young, Inc. 525 OLYMPIA AVENUE, SUITE 5 PUNTA GORDA, FLORIDA 33950 PHONE (352) 378-1444 WWW.GEORGEFYOUNG.COM ENGINEERING CERTIFICATE OF AUTHORIZATION NUMBER 21 CML, TRANSPORTATION, SUBSURFACE & STRUCTURAL ENGINEERING ECOLOGY   GIS   PLANNING   SURVEYING ST. PETERSBURG • LAKEWOOD RANCH • TAMPA • GAINESVILLE • LAKE WALES • PUNTA GORDA	No. DATE	Arcadia Stormwater and Flood Control Special Details INCLUDES PORTIONS OF: SECTIONS 25, 26, 31, 36, TOWNSHIP 37S., RANGE 24, 25E.	JOB NO. 21Y01018LC
							SHEET NO. SD47					

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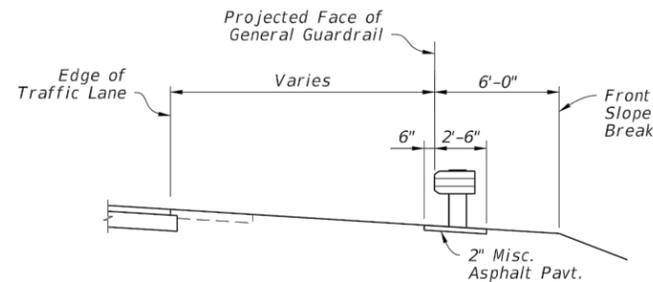
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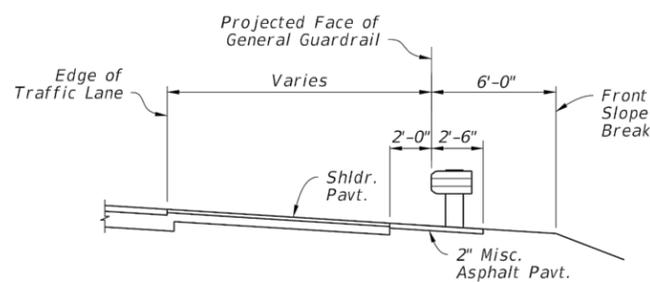
**APPROACH TERMINAL ASSEMBLY  
'PARALLEL' TYPE - PLAN VIEW**

**NOTES:**

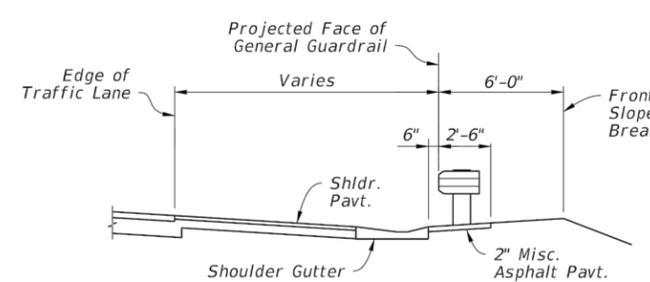
- INSTALLATION:** Locate Approach Terminals where called for in the plans, with the Post (1) placed at the Begin/End Guardrail Station indicated in the plans.  
  
The Plan Views shown herein are schematic only, showing basic geometry for Approach Terminals listed on the APL. The predefined Length of End Treatment, 'LE', includes the proprietary portion of various Approach Terminals and provides for more consistent planning of assembly installations across the differing Approach Terminal types. Forward-anchoring style Approach Terminals may vary from the planned lengths shown by up to 3'-0".  
  
Construct Approach Terminals as shown in the APL and in accordance with the manufacturer's unique drawing details, procedures, and specifications.  
  
Install posts in accordance with the manufacturer's drawings. The Special Posts on Sheet 24, including Special Steel Posts, Encased Posts, and Frangible Leave-Outs, are not permitted within the Approach Terminal segment unless otherwise called for in the plans.  
  
Align panel lap splices in accordance with the manufacturer's drawings, regardless of the direction of traffic.  
  
Install adjacent grading, gutters, and/or curbing as shown herein.
- GENERAL GUARDRAIL:** General Guardrail typically includes Panels and Post Spacing as shown on Sheet 2, including parallel and tapered segments.  
  
Approach Transitions, Low-Speed Guardrail, or Reduced Post Spacing Guardrail segments may be substituted for the General Guardrail shown herein if indicated in the plans.
- APPROACH TERMINAL TEST LEVEL:** Install either a Test Level 3 (TL-3) or Test Level 2 (TL-2) Approach Terminal as specified in the plans. TL-3 Approach Terminals may substitute for TL-2 Approach Terminals unless the substitution is specifically prohibited in the plans. TL-2 Approach Terminals may not substitute for TL-3 installations.
- IMPACT HEAD END DELINEATOR:** Apply Yellow Retroreflective Sheeting to the nose of the End Terminal in accordance with Specification 536.
- 2" MISCELLANEOUS ASPHALT PAVEMENT:** The Plan View depicts the Unpaved Shoulder condition. For Fully Paved Shoulder and Shoulder Gutter conditions, extend the 2" Misc. Asphalt Pavement as shown in the corresponding 'Section at Post (1)' details below.  
  
The 2" Misc. Asphalt Pavement shown upstream of Post (1) may be substituted with a different pavement type where called for in the Plans.
- CLEAR AREA REQUIREMENT:** Do not place any permanent aboveground installations within the areas shown with 1:10 maximum grading. For the finished condition, keep this area free of all aboveground obstructions, including dense vegetation and trees.
- 'CURBED' AND 'DOUBLE FACED' GUARDRAIL SEGMENTS:** See Sheet 8.



**SECTION AT POST (1)  
WITH UNPAVED SHOULDER**



**SECTION AT POST (1)  
WITH FULLY PAVED SHOULDER**



**SECTION AT POST (1)  
WITH SHOULDER GUTTER**

**END TREATMENT - APPROACH TERMINAL GEOMETRY - PARALLEL**

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DRAWN	PCS				
CHECKED	MP				
QUALITY CHK					
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City Of Arcadia  
P.O. Drawer 1000  
Arcadia, Florida, 34265  
(863) 494-4114



**George F. Young, Inc.**  
525 OLYMPIA AVENUE, SUITE 5 PUNTA GORDA, FLORIDA 33950  
PHONE (352) 378-1444 WWW.GEORGEFYOUNG.COM  
ENGINEERING CERTIFICATE OF AUTHORIZATION NUMBER 21  
CIVIL, TRANSPORTATION, SUBSURFACE & STRUCTURAL ENGINEERING  
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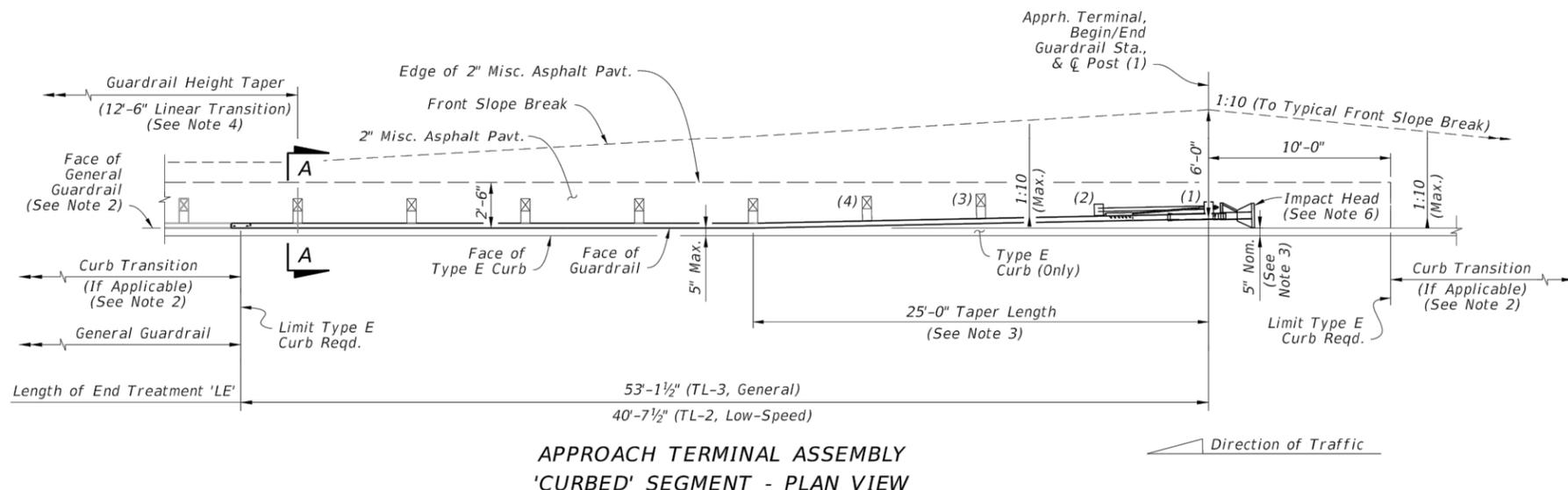
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**Arcadia Stormwater and Flood Control**  
Special Details  
INCLUDES PORTIONS OF:  
SECTIONS 25, 26, 31, 36, TOWNSHIP 37S., RANGE 24, 25E.

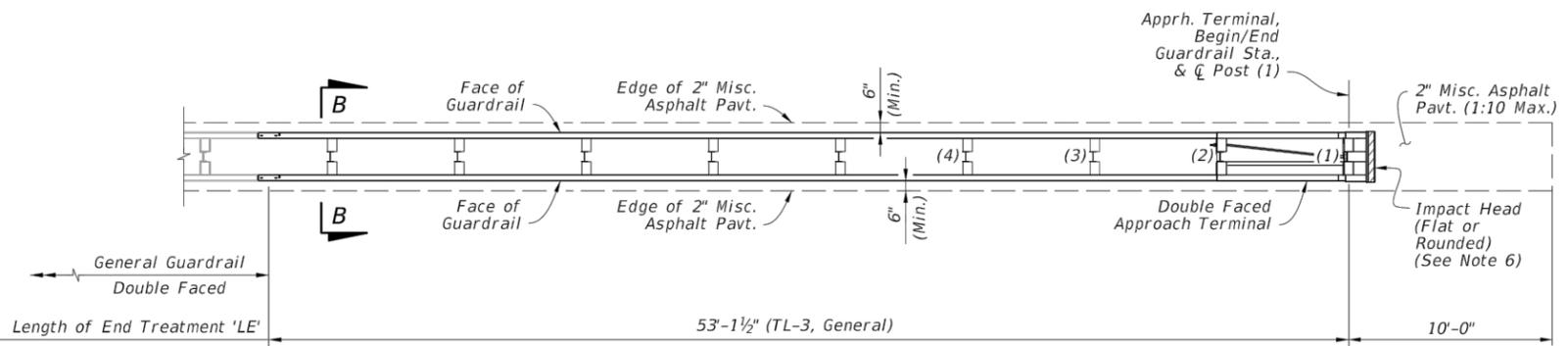
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SD48

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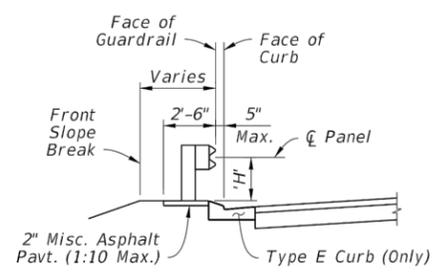
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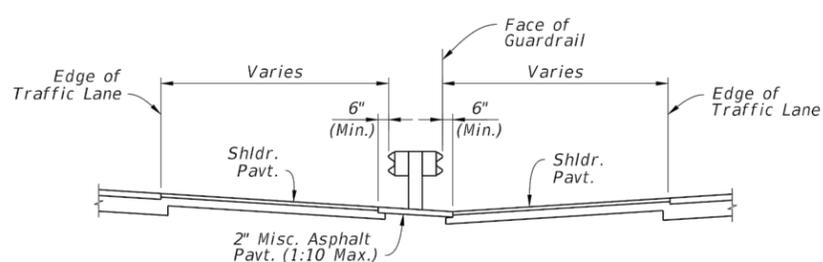
APPROACH TERMINAL ASSEMBLY  
'CURBED' SEGMENT - PLAN VIEW



APPROACH TERMINAL ASSEMBLY  
'DOUBLE FACED' SEGMENT - PLAN VIEW



'CURBED' SECTION A-A  
(Height, 'H', Measured from  
Misc. Asphalt Pavt.)



'DOUBLE FACED' SECTION B-B  
(1:10 Slope or Flatter Reqd.)

**NOTES:**

1. GENERAL: See Notes 1 through 3 on Sheet 7.
2. CURBED SEGMENTS: Type E curb is required within the limits shown. When a different curb type is called for outside of the Type E curb limits, transition the curb shape linearly, over a nominal distance ranging 5'-0" to 10'-0"
3. TAPER LENGTH: For Curbed Segments, taper the guardrail away from the roadway where shown to place the inside edge of the Impact Head at 5' behind the face of the curb. Where additional lateral offset is required to fit the Approach Terminal Assembly hardware, such as a soil plate, place the Impact Head as close to the curb as the hardware allows, not to exceed 2'-0" from the face of curb.
4. GUARDRAIL HEIGHT TAPER: For Curbed Segments, the connecting General Guardrail Mounting Height, 'H', is typically measured from the Lip of Gutter (See Sheet 6 Guardrail Sections, 'Adjacent to Curb'), while the End Terminal Assembly 'H' is measured from the Misc. Asphalt Pavt. (See Section A-A). Linearly taper the difference in Mounting Height over a minimum length of 12'-6", starting where indicated herein.
5. DOUBLE FACED SEGMENT: Connect to Double Faced General Guardrail. Use consistent Posts and Offset Block types as specified in the APL drawings over the entire Length of End Treatment, 'LE'. Posts and Offset Blocks in the adjoining General Guardrail segment may be different from those inside of the 'LE'. A change in post type between timber and steel is permitted, immediately outside of the 'LE' segment.  
  
Maintain the 1:10 maximum grading as shown in Section B-B throughout segment 'LE'. Where required, transition to differing adjacent slopes linearly, over a minimum longitudinal length of 25'-0".
6. IMPACT HEAD END DELINEATOR: Apply Yellow Retroreflective Sheeting to the nose of the End Terminal in accordance with Specification 536.
7. CLEAR AREA REQUIREMENT: Do not place any permanent aboveground installations within the areas shown with 1:10 maximum grading. For the finished condition, keep this area free of all aboveground obstructions, including dense vegetation and trees.
8. 2" MISCELLANEOUS ASPHALT PAVEMENT: The 2" Misc. Asphalt Pavement shown upstream of Post (1) may be substituted with a different pavement type where called for in the Plans.
9. SINGLE FACED 'PARALLEL' SEGMENTS: See Sheet 7.

END TREATMENT - APPROACH TERMINAL GEOMETRY CURBED AND DOUBLE FACED

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Arcadia, Florida, 34265  
(863) 494-4114



**George F. Young, Inc.**  
525 OLYMPIA AVENUE, SUITE 5 PUNTA GORDA, FLORIDA 33950  
PHONE (352) 378-1444 WWW.GEORGEFYOUNG.COM  
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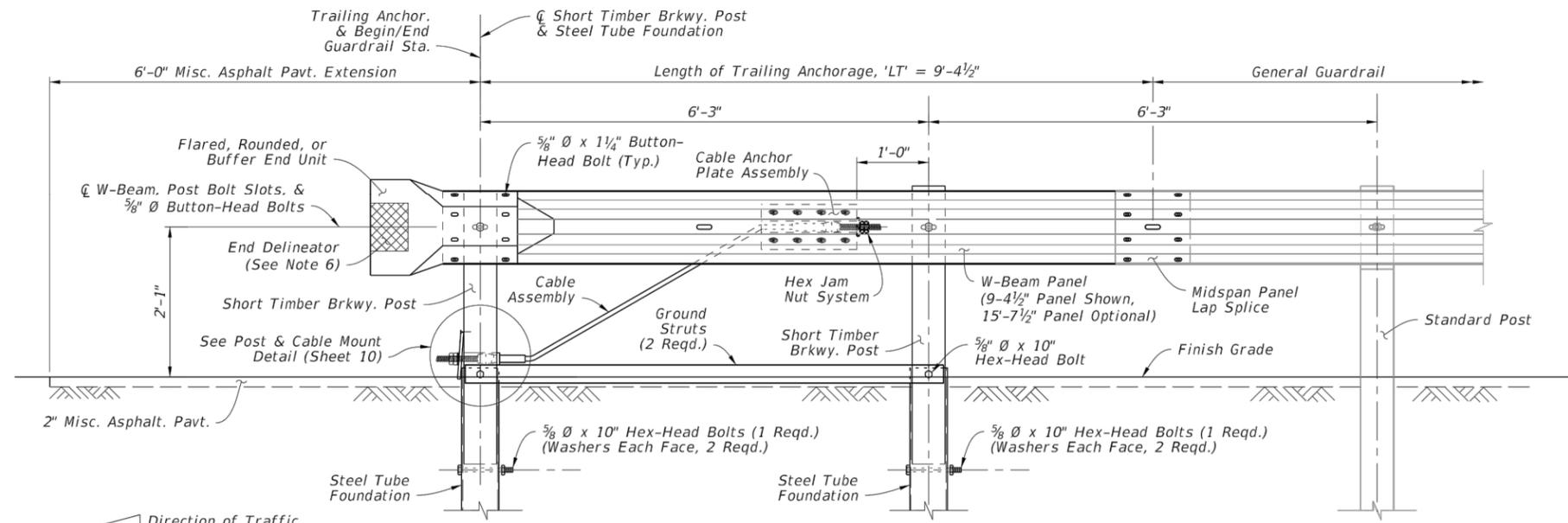
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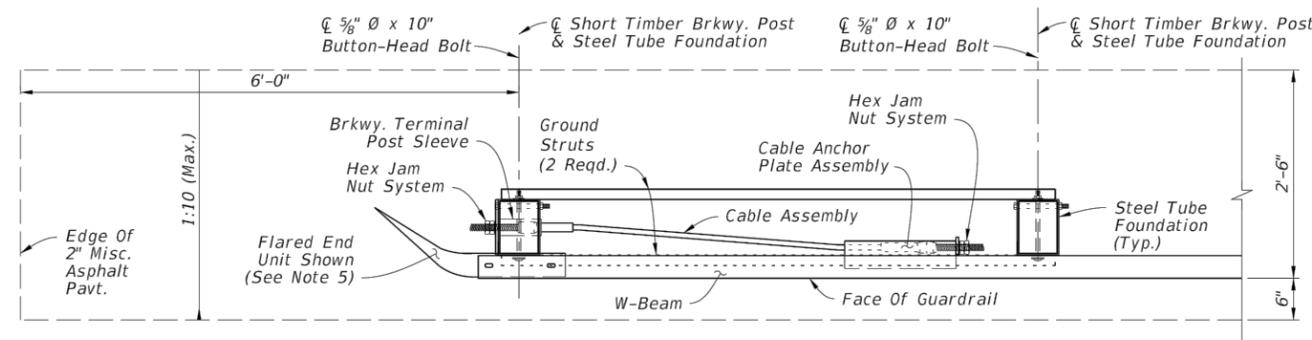
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SHEET NO.  
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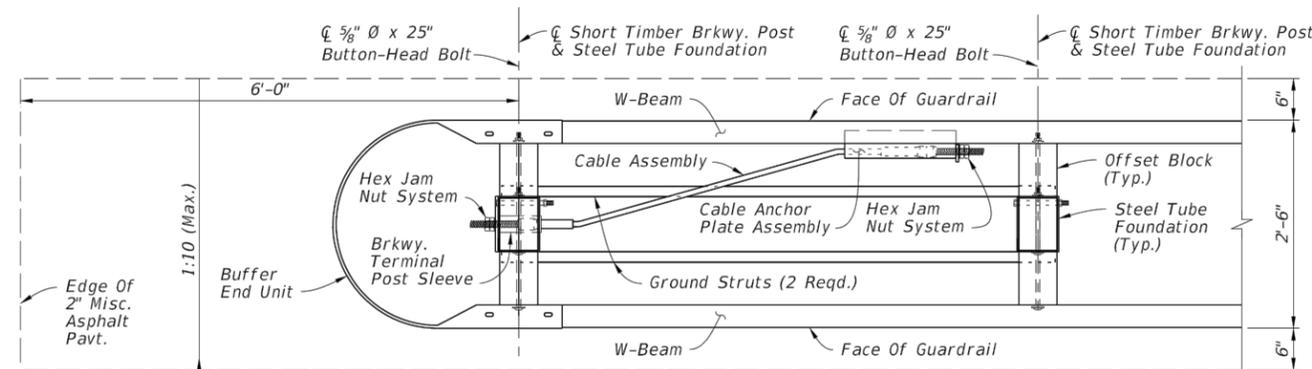
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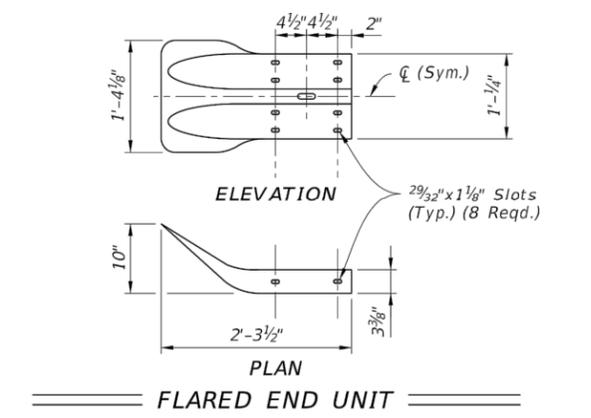
INSTALLED ELEVATION



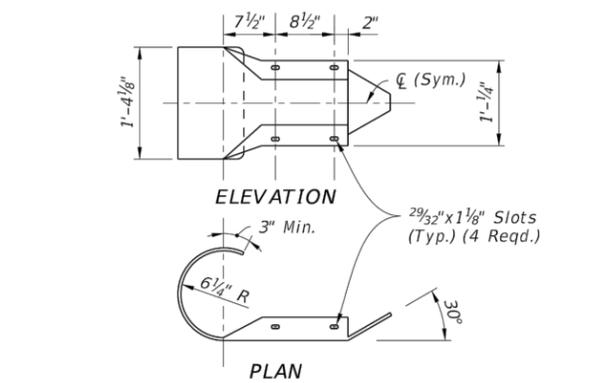
SINGLE FACE TRAILING ANCHORAGE INSTALLED PLAN



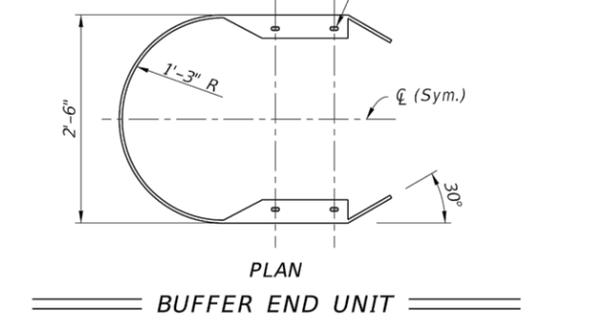
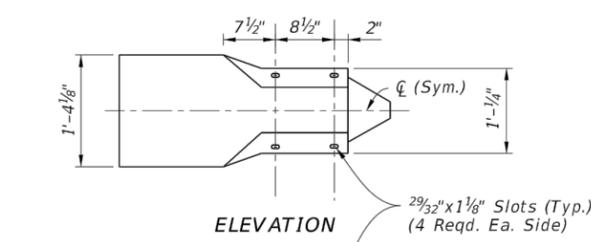
DOUBLE FACE TRAILING ANCHORAGE INSTALLED PLAN



FLARED END UNIT



ROUNDED END UNIT



BUFFER END UNIT

END TREATMENT - TRAILING ANCHORAGE

NOTES:

- COMPONENT DETAILS: For additional component details, See Sheet 10.
- END UNITS: Use materials for end units as defined in Specifications Section 967. End Units are referred to as "End or Buffer Sections" in AASHTO M180.  
Lap the Flared End Unit behind the W-Beam; lap the Rounded and Buffered End Units over the face of the W-Beam.
- FOUNDATIONS: Install Steel Tubes by either of the following methods:
  - Excavate, backfill, and compact material to provide full passive soil resistance to the surface of the Tube.
  - Drive the Tube using a dummy timber post to prevent damage to the Breakaway Post.
- GENERAL GUARDRAIL: General Guardrail typically includes Panels and Post Spacing as shown on Sheet 2, including parallel and tapered segments. Transitions, Low-Speed Guardrail, or Reduced Post Spacing Guardrail segments may be substituted for the General Guardrail shown herein if indicated in the plans.
- SIDEWALK REQUIREMENTS: When sidewalks are located adjacent to the End Treatment, install a Rounded End Unit (Flared End Unit not permitted for this case).
- END DELINEATOR: Mount retroreflective sheeting to the approach face of the End Unit in accordance with Specification Sections 536 and 967.

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